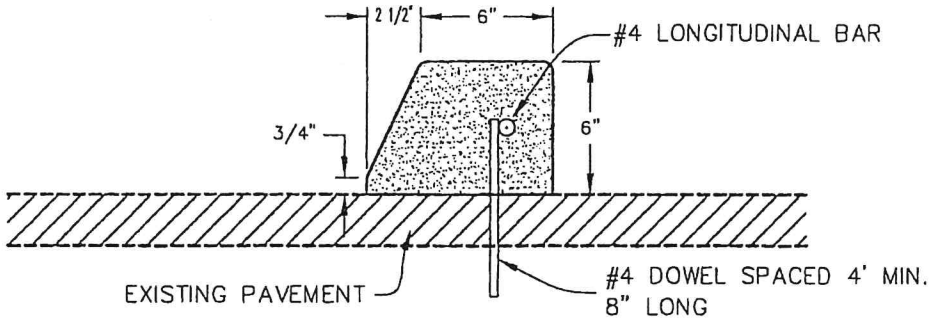
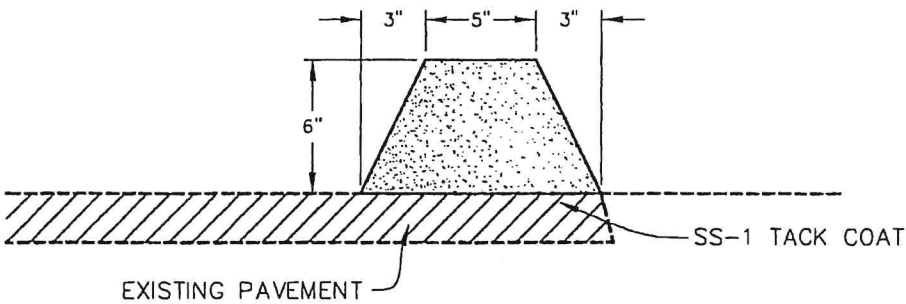


6" P.C.C. CURB (TYPE A)



6" P.C.C. CURB (TYPE B)



6" A.C. DIKE

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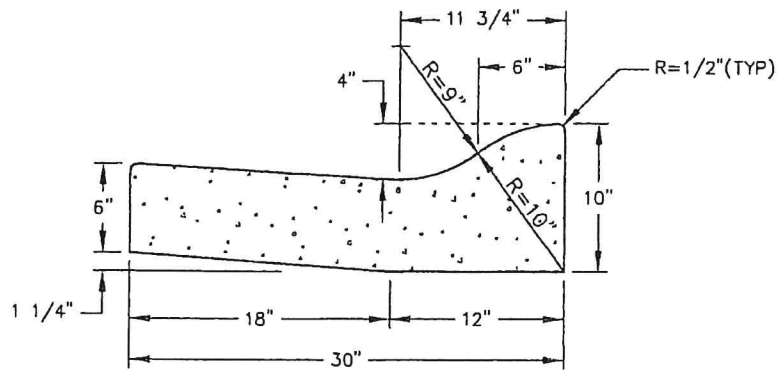
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 CHECKED BY: KGS III SCALE: NONE
 APPROVED: *K.G. Skill III*

6" CURB AND
 6" ASPHALT DIKE

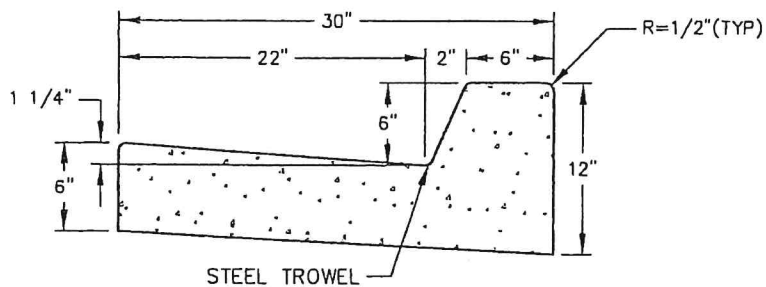
STANDARD DETAIL

201

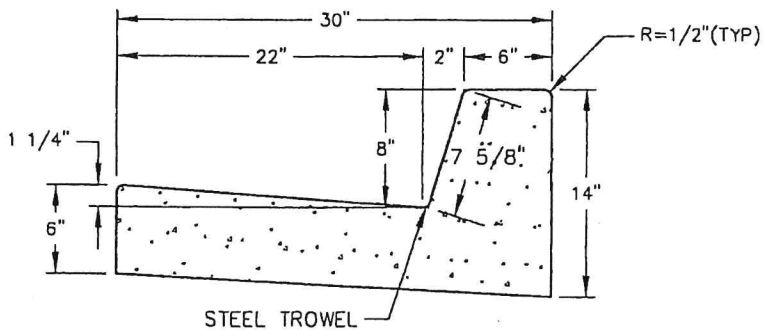
SHEET 1 OF 1



ROLL CURB



6" BARRIER CURB



8" BARRIER CURB

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DRAWN BY: CAD DATE: JAN,09

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APPROVED: *K. G. Still III*

CURB
AND GUTTER

STANDARD DETAIL

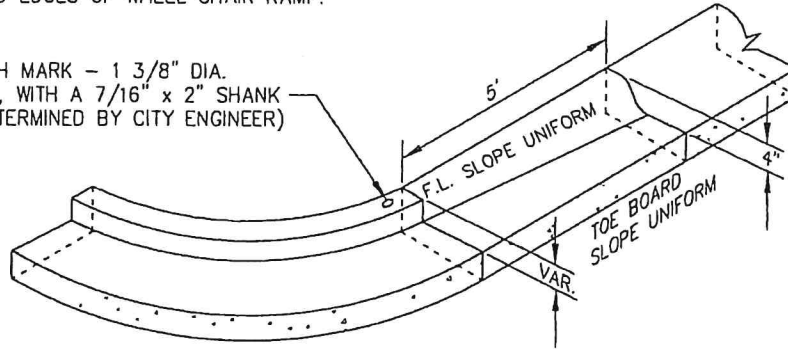
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SHEET 1 OF 1

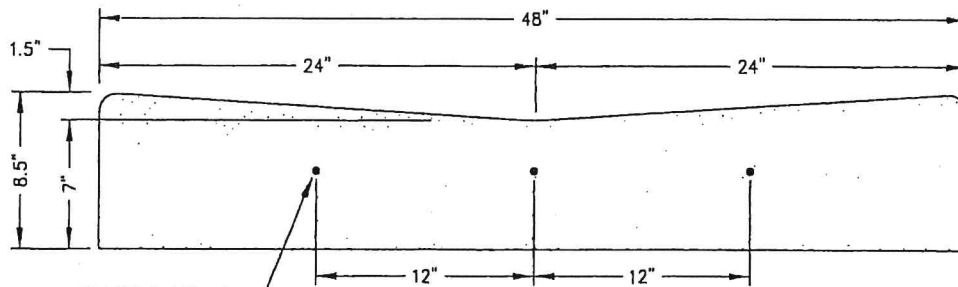
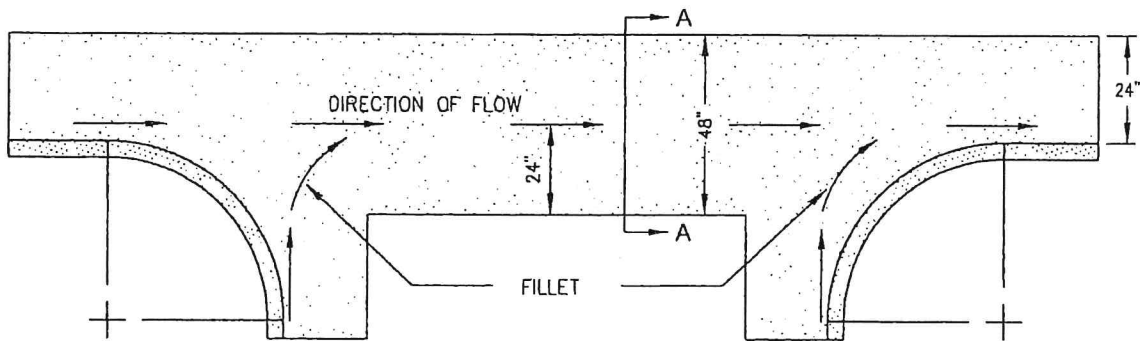
NOTE

VERTICAL CURB & GUTTER TO BE USED AT ALL CURB RETURNS WITH 1/4" EXPANSION JOINT AT BOTH ENDS OF CURB RETURN AND EDGES OF WHEEL CHAIR RAMP.

BRONZE BENCH MARK - 1 3/8" DIA.
CROWNED TOP, WITH A 7/16" x 2" SHANK
(LOCATION DETERMINED BY CITY ENGINEER)



BARRIER C & G TO ROLL C & G TRANSITION



PLACE 3 NO. 4 REBAR 12" O.C.

SECTION A-A

CROSS GUTTER

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STANDARD DETAIL

203

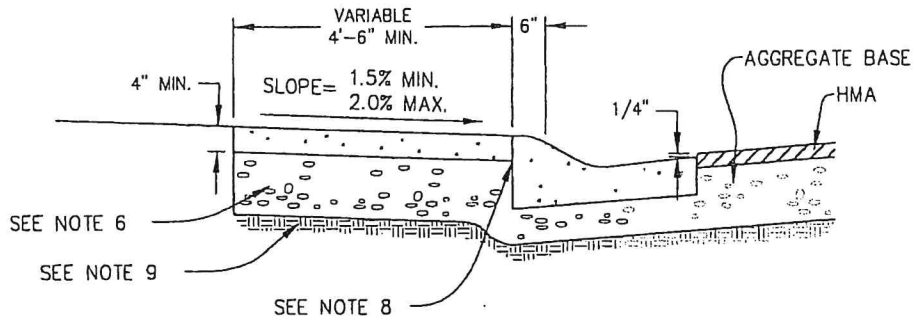
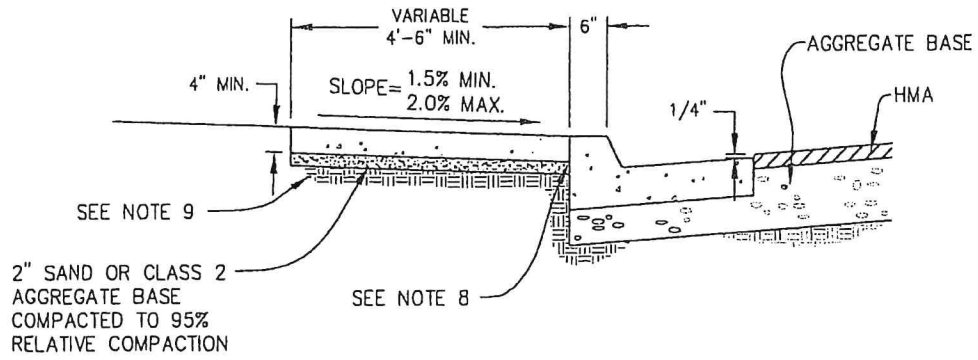
SHEET 1 OF 1

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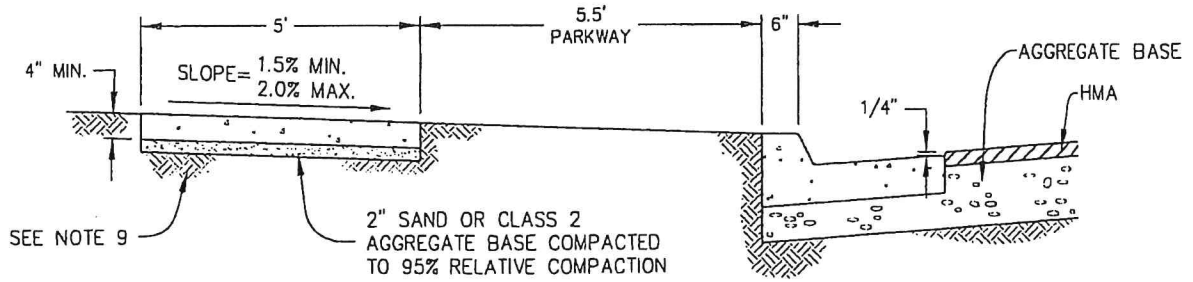
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APPROVED: *[Signature]*

CROSS GUTTER AND CURB AND GUTTER TRANSITION



CONTIGUOUS TYPE



SEPARATED TYPE

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DRAWN BY: CAD DATE: MAR,14

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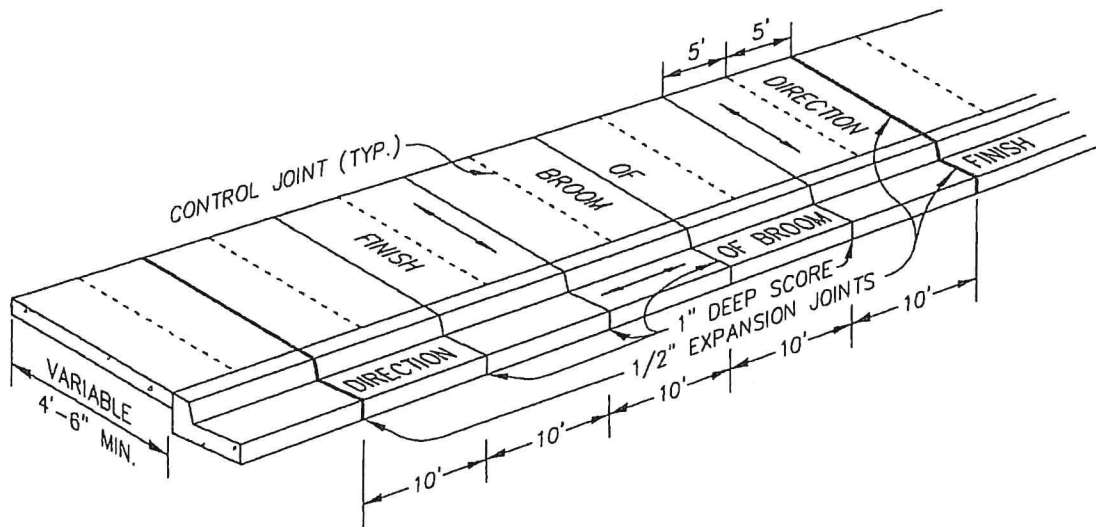
APPROVED: *[Signature]*

SIDEWALK, CURB
AND GUTTER

STANDARD DETAIL

204

SHEET 1 OF 2



CONTROL JOINT LAYOUT

NOTES

1. ALL CONCRETE SHALL BE CLASS B P.C.C.
2. 1/2 INCH, PRE-MOLDED JOINT FILLER SHALL BE INSTALLED IN EXPANSION JOINTS AT REGULAR INTERVALS NOT EXCEEDING 50 FEET, AT THE B.C. AND E.C. OF ALL CURB RETURNS AND AT THE END OF ALL DRIVEWAYS, AND SHALL BE FULL-DEPTH AND COMPLETELY FILL THE JOINT.
3. A MINIMUM OF 2 INCHES OF SAND, OR CLASS 2 AGGREGATE BASE, TO BE PLACED UNDER THE SIDEWALK. (SEE NOTE 6 BELOW)
4. ALL WORK DONE AND ALL MATERIALS SUPPLIED SHALL CONFORM TO THE ORLAND IMPROVEMENT STANDARDS.
5. THE CONTRACTOR SHALL NOTIFY THE CITY ENGINEER FOR INSPECTION AT LEAST 24 HOURS PRIOR TO PLACING CONCRETE.
6. FOR SIDEWALK ABUTTING ROLLED CURB AND GUTTER, THE THICKNESS OF AGGREGATE BASE UNDER THE SIDEWALK SHALL BE THE SAME AS THE THICKNESS PLACED UNDER THE STREET PAVEMENT.
7. EXPANSION JOINTS IN SIDEWALK SHALL BE ADJACENT TO EXPANSION JOINT IN CURB AND GUTTER.
8. PROVIDE COLD JOINT AT BACK OF CURB. IF CURB, GUTTER, AND SIDEWALK ARE POURED MONOLITHICALLY, PROVIDE 1" DEEP SCORE AT BACK OF CURB.
9. SUBGRADE UNDER SIDEWALK COMPACTED TO 92% RELATIVE COMPACTION.

CITY OF ORLAND

STANDARD DETAIL

204

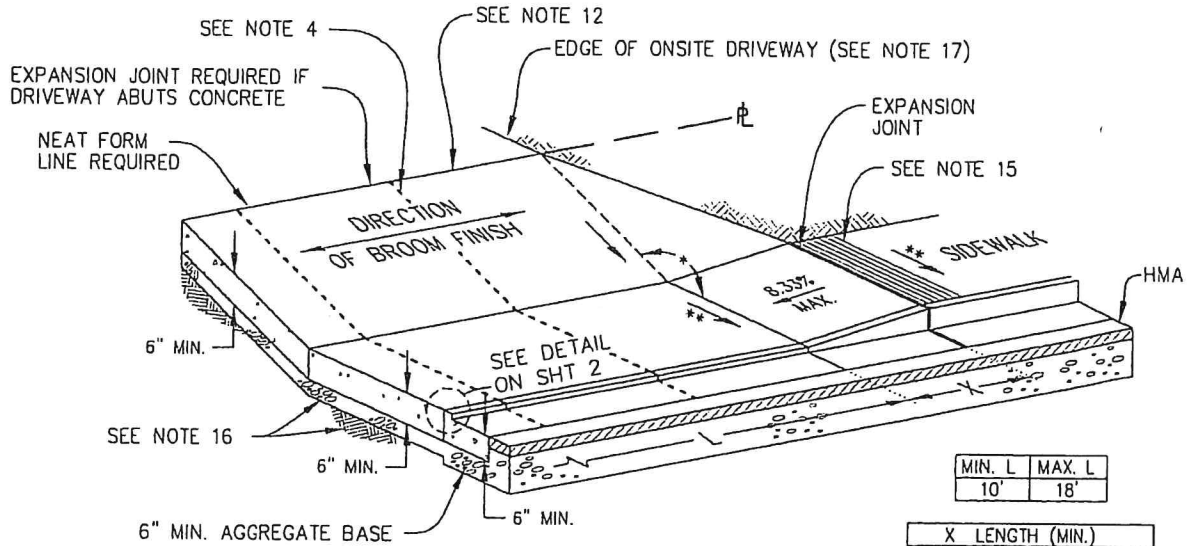
SHEET 2 OF 2

DRAWN BY: CAD DATE: MAR,14

CHECKED BY: KGS III SCALE: NONE

APPROVED: *K. G. St. Pierre III*

**SIDEWALK, CURB
AND GUTTER**

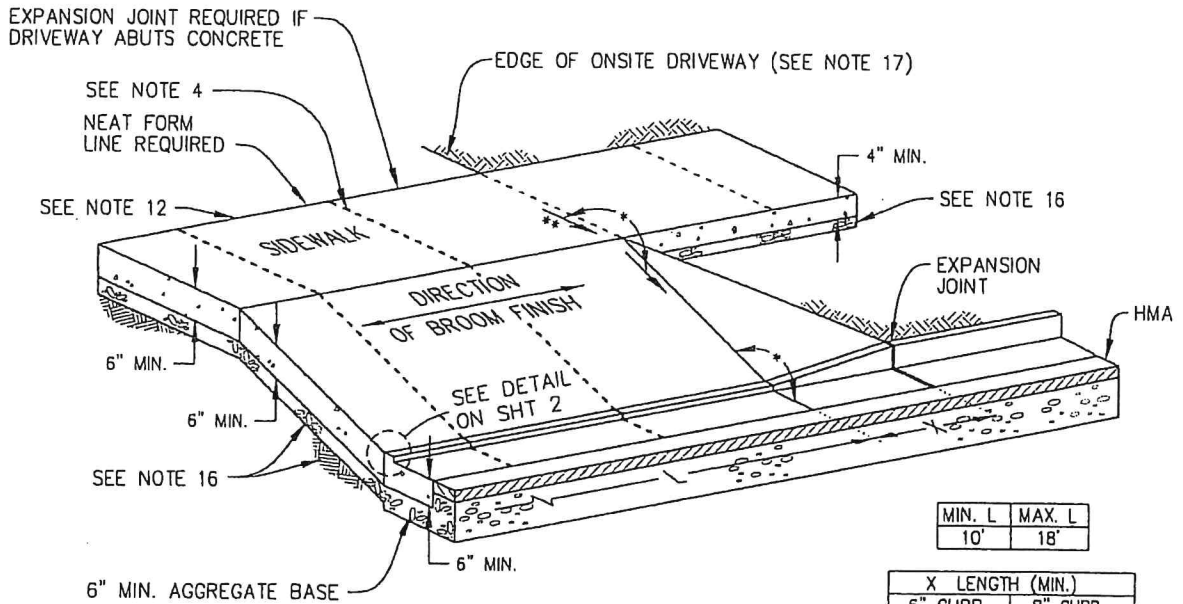


MIN. L	MAX. L
10'	18'

X LENGTH (MIN.)	
6" CURB	8" CURB
5.5'	7.5'

- * MAX. ALGEBRAIC DIFFERENCE OF 17.5%
- ** SIDEWALK SLOPE SHALL BE A MINIMUM OF 1.5% AND SHALL NOT EXCEED 2%

CONTIGUOUS SIDEWALK



MIN. L	MAX. L
10'	18'

X LENGTH (MIN.)	
6" CURB	8" CURB
5.5'	7.5'

- * MAX. ALGEBRAIC DIFFERENCE OF 17.5%
- ** SIDEWALK SLOPE SHALL BE A MINIMUM OF 1.5% AND SHALL NOT EXCEED 2%

SEPARATED SIDEWALK

CITY OF ORLAND

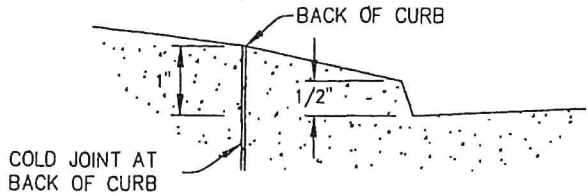
DRAWN BY: CAD DATE: MAR,14
 CHECKED BY: KGS III SCALE: NONE
 APPROVED: *K. G. Slade III*

**RESIDENTIAL
DRIVEWAY**

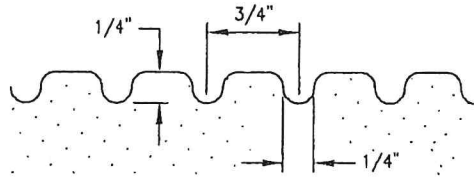
STANDARD DETAIL

205

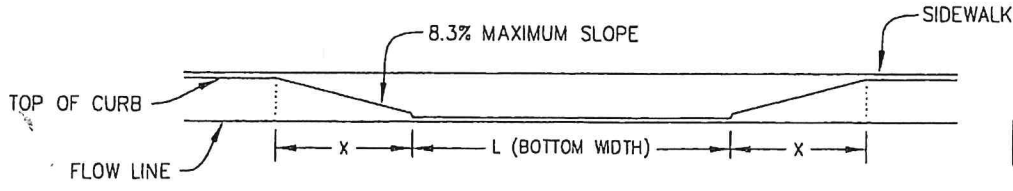
SHEET 1 OF 2



DETAIL



GROOVE DETAIL



ELEVATION

MIN. L	MAX. L
10'	18'

X LENGTH (MIN.)	
6" CURB	8" CURB
5.5'	7.5'

NOTES

1. ALL WORK TO BE DONE AND ALL MATERIALS TO BE SUPPLIED SHALL CONFORM TO THE ORLAND PUBLIC WORKS CONSTRUCTION STANDARDS.
2. ALL CONCRETE SHALL BE CLASS B P.C.C.
3. THE AREA INCLUDED WITHIN THE SLOPES OF THE DRIVEWAY SHALL BE GIVEN A HEAVY BROOM FINISH AFTER BEING TROWELED.
4. CONTROL JOINTS SHALL EXTEND FROM LIP OF GUTTER TO THE BACK OF SIDEWALK UNLESS OTHERWISE SPECIFIED. CONTROL JOINTS SHALL BE EVENLY SPACED AT A MAXIMUM INTERVAL OF 8 FEET.
5. TOP OF LIP AT THE FLOWLINE TO BE TROWELED STRAIGHT AND TRUE.
6. WHERE CURB IS EXISTING AND NO DEPRESSION HAS BEEN PROVIDED, THE EXISTING CURB SHALL BE REMOVED TO THE FIRST EXPANSION JOINT BEYOND EITHER SIDE.
7. WHERE AN EXISTING SIDEWALK IS IN PLACE, IT SHALL BE REMOVED TO THE FIRST EXPANSION JOINT BEYOND EITHER SIDE.
8. ALLEY CURB RETURNS MAY BE DEPRESSED AS PART OF THE DRIVEWAY ONLY WHEN APPROVED BY THE CITY ENGINEER.
9. DRIVEWAYS SHALL NOT BE CONSTRUCTED CLOSER THAN 20 FEET TO THE END OF STREET CURB RETURNS UNLESS APPROVED BY THE CITY ENGINEER.
10. THE MINIMUM LENGTH OF FULL HEIGHT CURB BETWEEN DRIVEWAYS ON THE SAME LOT SHALL BE 24 FEET.
11. THE MINIMUM LENGTH OF FULL HEIGHT CURB BETWEEN DRIVEWAYS ON ADJACENT LOTS SHALL BE 6 FEET.
12. ONSITE GRADING MAY BE REQUIRED TO ELIMINATE EXCESSIVE GRADE CHANGE AND TO MAINTAIN SUITABLE DRAINAGE.
13. MAXIMUM CURB OPENING MAY BE INCREASED DUE TO SPECIAL CONDITIONS WITH APPROVAL OF THE CITY ENGINEER.
14. DRIVEWAY APPROACH SHALL BE POURED SEPARATELY FROM CURB UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
15. SIDEWALK ADJACENT TO THE TOP OF RAMPS SHALL HAVE A 12" WIDE GROOVED BORDER STRIP WITH 1/4" GROOVES AT 3/4" ON CENTER, SEE GROOVE DETAIL.
16. 2" SAND OR CLASS 2 AGGREGATE BASE COMPACTED TO 95% RELATIVE COMPACTION OVER SUBGRADE COMPACTED TO 92% RELATIVE COMPACTION.
17. BOTTOM WIDTH OF PROPOSED DRIVEWAY SHALL BE THE SAME AS THE ONSITE DRIVEWAY.

CITY OF ORLAND

DRAWN BY: CAD DATE: MAR,14
 CHECKED BY: KGS III SCALE: NONE
 APPROVED: *K.G. Spivey III*

RESIDENTIAL DRIVEWAY

STANDARD DETAIL

205

SHEET 2 OF 2